



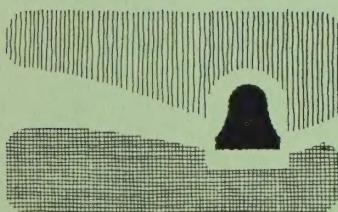
# HIGUERA COMMERCE PARK

## specific plan

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## city of san luis obispo

Department of Community Development  
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ARCHITECTURAL GUIDELINES FOR

HIGUERA COMMERCE PARK

Site Planning

Building Mass and Design

Landscape Development

Signing

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**A. Site Plan**

1. Orient buildings to minimize wind at entries and work bays.
2. Orient buildings to take maximum advantage of active and passive solar opportunities.
3. Provide defined pedestrian access from street and parking lot to major buildings.
4. Vehicle parking at front of lot between building and street should be avoided.
5. Use zero lot line building placement whenever possible. Narrow strips between buildings and property lines are generally not acceptable.
6. Site plan should encourage water conservation, eg. by minimizing runoff and retention of on-site drainage.
7. Building placement/site design should allow for all necessary site uses with a minimum of conflict.
8. Material transitions shall occur logically. Concrete curbs, mow strips, and paving/walks shall provide variety of ground plane textures with safety and ease of maintenance.
9. Use of fences and walls should be minimized except where required for screening outdoor storage. When proposed, walls/fences shall be solid, attractive, two-sided, and designed for low maintenance with materials and/or colors complimentary to building.
10. Utility meters, roof-mounted equipment and utility pads and boxes should be screened.

**B. Building Design**

1. Large roof and wall planes unrelieved by shadow or textural interest are generally not acceptable.
2. Building design should be unified and emphasize single, larger scale structures rather than multiple small, detached structures on site.

3. All roof mounted equipment including HVAC units, shall be screened as an integral part of design. Solar collectors need not be screened but shall comply with "Guidelines for Roof-Mounted Solar Collectors" available at Community Development Department.
4. No specific architectural designs or styles or materials are endorsed for Higuera Commerce Park, however compatibility with adjacent sites and structures is required. The commission encourages innovation in design and materials appropriate to commercial/industrial uses.

C. Landscape Development

1. All parking areas shall be screened from public streets with 3 ft. high continuous berm and/or shrub/hedge planting.
2. Interior yards need not be landscaped unless required by the Architectural Review Commission. However, all parking lots shall be landscaped with ground cover, shrubs, and trees for color, texture, shade, and for safe pedestrian/vehicular orientation.
3. Drought-resistive landscape plantings with low maintenance characteristics are encouraged. California native plants are especially encouraged within Higuera Commerce Park.
4. Plantings should be selected and placed to reinforce and enhance pedestrian scale and character along interior street frontages. Trees which provide shade canopies or seasonal color are encouraged.
5. Plantings shall be selected for their natural (unpruned) ability to screen undesirable views and wind, control and direct solar exposure, and complement functional needs of site.

D. Signs

1. Project directory and tenant signing is generally required and should be integrated with building design for maximum safety, convenience, and design compatibility.
2. Low monument signs are encouraged for all tenant and directory signs. Pole signs should be avoided.
3. Sign colors, materials and design should match or complement building design and materials.

HIGUERA COMMERCE PARK SPECIFIC PLAN

City of San Luis Obispo

Adopted: July 17, 1979 (Resolution No. 3912)

Amended: February 2, 1982 (Resolution No. 4732)

September 20, 1983 (Resolution No. 5225)

April 3, 1986 (Resolution No. 5916)

1. 277-278 279-280 281-282  
2. 283-284 285-286 287-288  
3. 289-290 291-292 293-294  
4. 295-296 297-298 299-300



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## INTRODUCTION AND SUMMARY

### Purpose of this Specific Plan

This specific plan provides for the development of an 80-acre site located on the east side of South Higuera Street between Prado Road and Tank Farm Road (see vicinity map). The plan is based on, and is a refinement of, the General Plan Land Use Element of the City of San Luis Obispo. It outlines land use and property development standards and proposals for circulation, utilities, and other facilities which compose a schematic subdivision plan for the site.

### Summary of the Plan

The Higuera Commerce Park Specific Plan prescribes -- more precisely than the general plan -- appropriate land use, provisions for streets, drainage and utilities improvements, design and property development standards, and other measures which will insure the orderly and timely implementation of the General Plan.

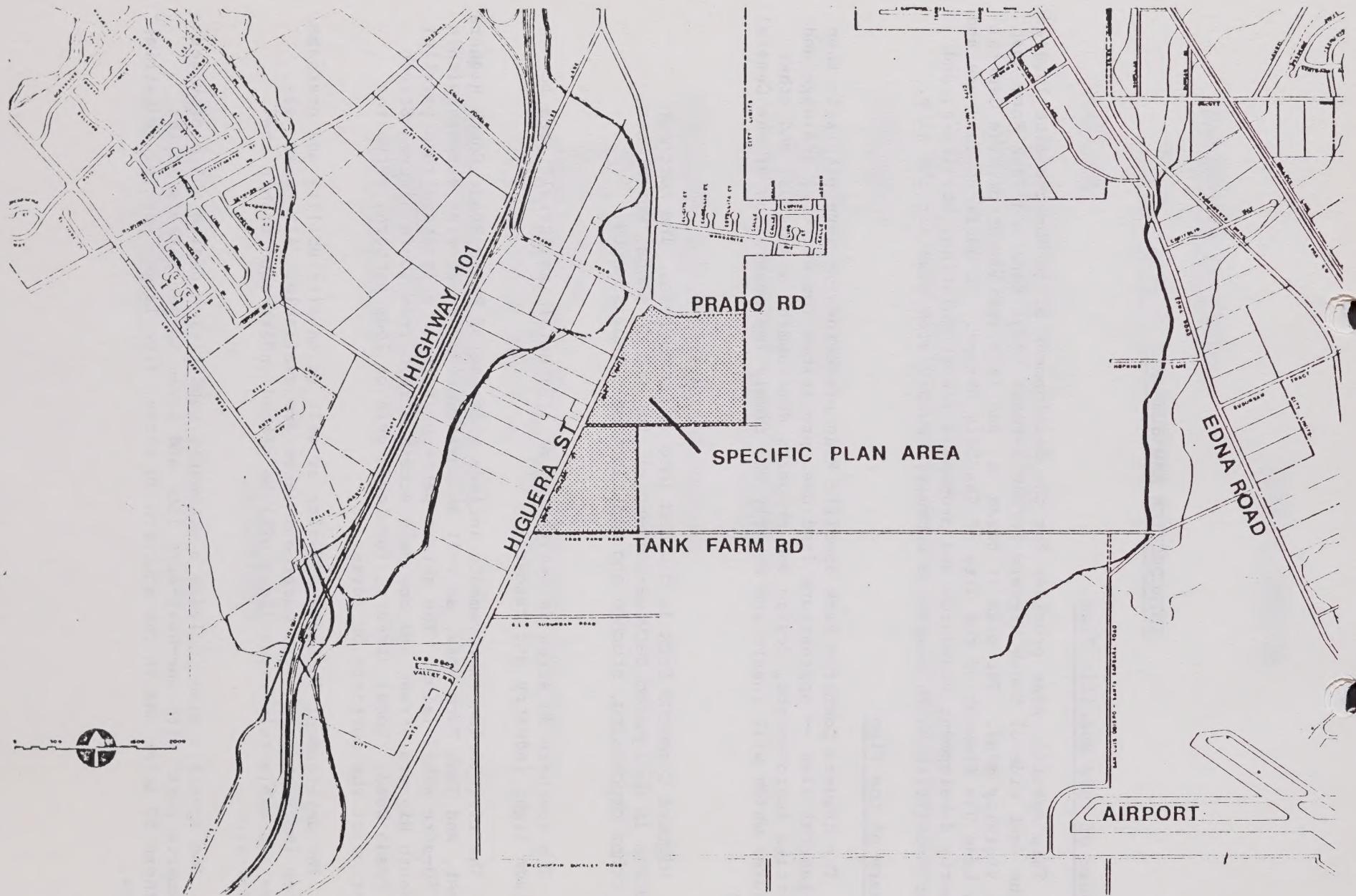
Higuera Commerce Park is divided into two sub-areas. The northern 50 acres is designated service-commercial, to include shops, warehouses, and other processing, storage and distribution establishments.

The southern 30 acres is designated as a "special industrial" area to include light industry and research.

The circulation improvements include widening of Prado Road, South Higuera Street, and Tank Farm Road, as well as construction of interior streets within the 50-acre subdivision. The street pattern includes a frontage road parallel to South Higuera Street and one main access street from South Higuera Street and Prado Road. Local streets, forming a grid or loop pattern, serve the interior of the northern 50 acres.

The development includes off-site as well as on-site utility and drainage system improvements. City water and sewer are available. All power, gas, phone and cable television lines will be placed underground within the subdivision.

The specific plan includes a schematic subdivision design which provides a "commerce park" with one-half-acre lots and seven larger parcels in the northern 50 acres and in the southern 30 acres, five large "special industrial" sites.



## VICINITY MAP

## LAND USE

### Zoning Regulations

This specific plan supersedes the provisions of the Zoning Regulations. If the specific plan does not contain a particular type of standard, the conventional zoning standards shall apply.

### Specific Plan Land Use Proposals

The specific plan is consistent with the general plan and the intent of the C-S and M zones. But, through the SP overlay zone, the specific plan provides more detailed definition of land uses, property development, design and performance standards particularly suited to this site.

### Land Use Categories

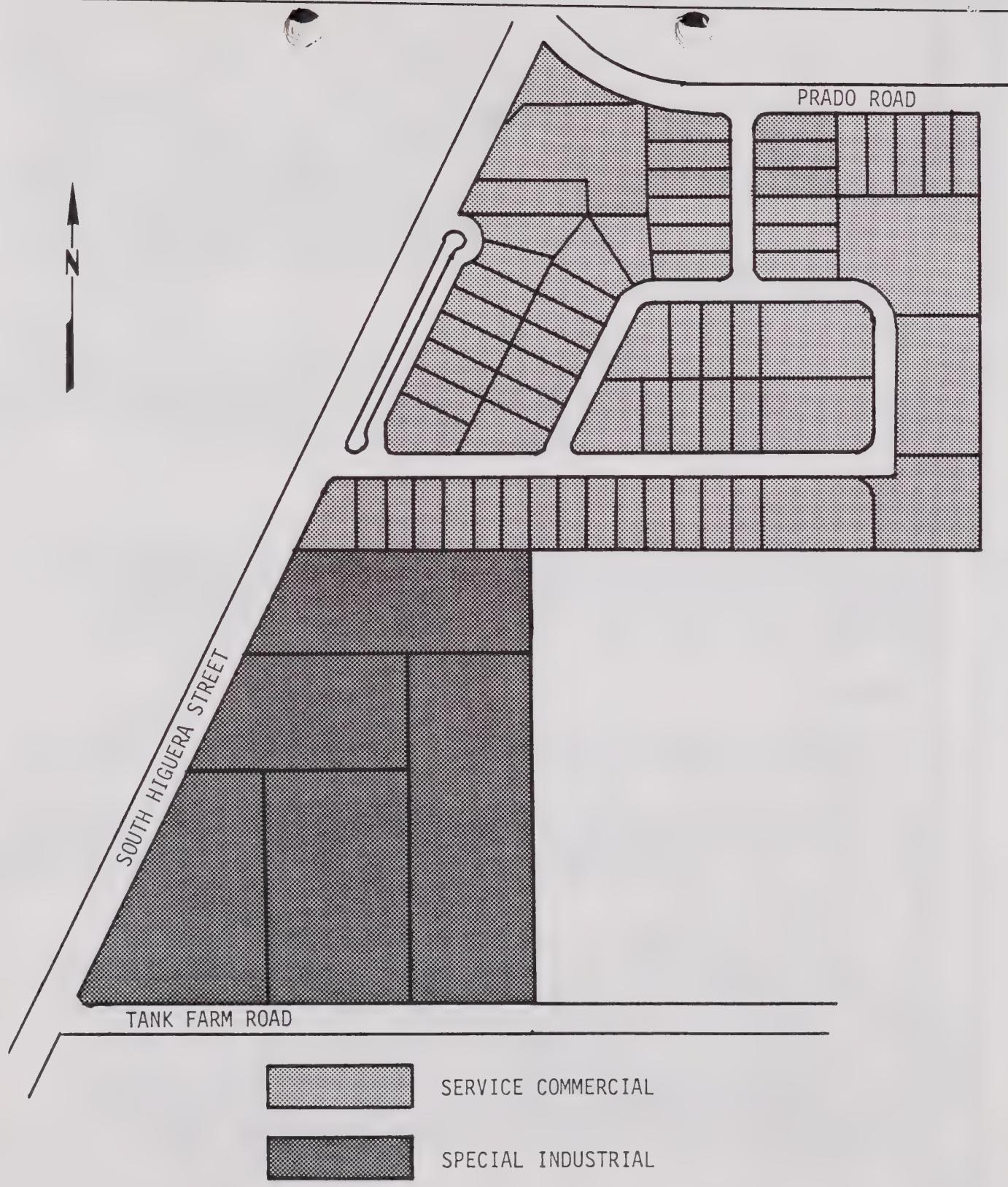
The land use map on the following page shows the total planning area divided into two sub-areas: the service-commercial district and the special industrial area (see Specific Land Use Plan). Land uses which should be developed within each of these areas are listed on the following pages.

### Allowed Uses

Under this category, a wide range of uses are allowed and no city review of new uses is required--within each of the sub-areas.

Conditional Uses are those which generally meet the intent of each of the plan's sub-areas but which may be incompatible with allowed uses. Different locations or special conditions may be desired. Administrative use permit procedures of the Zoning Regulations shall apply to all conditional uses listed in this specific plan.

Prohibited uses are those considered inappropriate within this development, primarily due to the area's classification as an "approach and climb-out extension area" within the San Luis Obispo Airport Land Use Plan. Certain uses are considered incompatible with airport operations and are thus explicitly prohibited by this plan: churches, clubs and lodges; gasoline service stations; outdoor theatres, athletic fields or grounds involving assembly; radio or television antennae and transmitters; residential uses (except caretaker's quarters); schools, hospitals or other institutions. Utility company offices and regional administrative offices are also prohibited.



HIGUERA COMMERCE PARK SPECIFIC PLAN

SPECIFIC LAND USE

### The Service-Commercial District

The service-commercial district provides for most uses allowed or conditionally allowed by the C-S (service-commercial) zone. Most of these uses will be conducted within a building with related storage yards adequately landscaped and screened from prominent street views.

Uses in this sub-area shall be allowed or conditionally allowed, as provided in the Zoning Regulations, C-S zone; exception: those uses listed previously in this specific plan as prohibited uses shall not be allowed.

### Special Industrial District

The southern 30 acres of the specific plan area are designated "special industrial." This sub-area is intended to accommodate primarily clean, light industries -- such as research and development, laboratories, precision manufacturing -- which require substantial space as well as city services, and which will draw most of their employees from the local labor market.

Allowed Uses: Laboratories and other technical facilities for research, testing and processing; electronic, optical, pharmaceutical and other precision products, parts, instruments or equipment, or similar manufacturing; public utility, construction engineering, soils testing and similar design, engineering, and testing facilities; printing and publishing firms, accessory offices, machine shops, cafeterias, recreation rooms and similar incidental uses or support facilities serving the employees of allowed uses; agricultural uses including cultivation of field, vine or tree crops, pasture or grazing, and other outdoor agricultural activities.

Conditional Uses: Any other large-scale, light-industrial use allowed or conditionally allowed in the M zone, operated predominantly within a building and having physical appearance and functional characteristics similar to the above allowed uses; greenhouses, storage, packing or processing of agricultural products produced on-site, located inside buildings.

## Specific Property Development, Performance and Design Standards

The specific plan includes special property development standards applicable to the entire planning area and outlined below. Some of these standards are more restrictive than conventional zoning and they prescribe additional design and development controls considered appropriate to this planned commerce park setting.

### 1. Maximum Height

The maximum height of buildings (including mechanical appurtenances, special equipment service structures, and architectural features) is 35 feet.

Only if specifically approved by the Architectural Review Commission can appurtenances, equipment, service structures, and architectural features be extended to a maximum height of 50 feet.

### 2. Lot Area

All lots in the northern 50 acres shall have a minimum area of 12,000 square feet but shall not exceed a maximum area of five acres. Each lot in the southern 30-acre special industrial sub-area shall be at least four acres. The arrangement of lots shall be as shown on the Specific Land Use Map. Condominiums of any size may be approved pursuant to city subdivision regulations;

a. Where the total area to be developed and subdivided for condominiums conforms to the minimum lot area standard; and

b. Where it will meet all other property development standards (based on anticipated tenant composition).

### 3. Minimum Lot Frontage and Width

The minimum lot frontage and width in the northern 50-acre subdivision shall be 80 feet. The minimum lot frontage and width in the southern 30-acre special industrial sub-area shall be 300 feet.

### 4. Minimum and Maximum Lot Depth Ratio

The average lot depth shall not be less than 150 feet nor greater than a 4:1 ratio with average lot width.

### 5. Maximum Building Coverage

The maximum lot coverage of all buildings shall not exceed 50 percent of the total lot area, except where specifically approved by the Architectural Review Commission.

## 6. Landscaping

A minimum of 15 percent of total lot area shall be landscaped including setback areas, parking lots, and spaces adjacent to buildings.

## 7. Minimum Yards

The following standards shall apply to all lots within the specific planning area:

a. Street yards: the minimum required street yard shall depend on the height of the proposed building adjoining the street (within 15 feet of front line). The required street yard shall be landscaped except for necessary driveways.

ADJOINING BUILDING HEIGHT	MINIMUM YARD
Where no building adjoins	10 feet
Where building 20 feet high or less adjoins	15 feet
Where building more than 20 feet high adjoins	20 feet

b. Other yards: side and rear yards may be reduced to zero feet unless otherwise precluded by requirements of the city's building codes.

c. In the southern 30-acre special industrial sub-area, all buildings along South Higuera Street shall be set back at least 15 feet from the nearest pedestrian easement or street right-of-way line, or if farther, a distance equal to the height of the adjacent building.

## 8. Off-Street Parking

a. Parking requirement for each use shall be as required by the ordinance (Section 9202.5.F.4).

b. Where uses are unspecified, parking requirement shall be determined by the Community Development Director.

## 9. Off-Street Loading

The minimum size of a loading space is 12 feet by 40 feet.

For projects which exceed 10,000 sq. ft. of building area, one off-street loading space shall be provided.

The Community Development Director may require additional loading spaces for uses which have building areas larger than 10,000 square feet.

#### 10. Driveways

All driveways shall meet or exceed minimum standards defined in the city's Zoning Regulations. The Community Development Director may require wider driveways where needed to accommodate on-site truck circulation.

#### 11. Outdoor Storage

Facilities for outdoor storage shall be provided where appropriate to the proposed use. Uses which typically require outdoor storage include retail sales of building materials, contractor's yard, manufacturing, landscape nursery, auto repair and related services. Outdoor storage shall occur in designated areas only. Required parking areas and driveways shall not be used for outdoor storage or business activities (sales, service, etc.).

#### 12. Screening of Outdoor Areas

Outdoor loading and sales areas need not be screened unless specifically required by the Architectural Review Commission.

Outdoor storage shall be screened from general public visibility from Prado Road, South Higuera Street and Tank Farm Road.

On interior streets, vehicle parking and outdoor storage areas should be screened from street view, unless excepted from screening requirements by the Architectural Review Commission. Screening from side or rear view need not be provided unless specifically required by the Architectural Review Commission.

#### 13. Architectural Design Criteria

In addition to standard architectural design guidelines, Sign Regulations and other similar design standards adopted by the city, the following special criteria shall be observed:

a. Roof and exterior materials, lighting or electrical equipment use which might create glare, confusion or interference with airport operations, shall not be allowed. When doubt exists, the city shall refer the specific design proposal to the Airport Land Use Commission for comment and recommended conditions.

b. Noise-sensitive commercial or industrial uses, and any building or use involving public assembly shall be referred to the Airport Land Use Commission for review. Such uses may be approved, subject to specific conditions regarding location, noise attenuation or insulation, aviation easements and similar controls.

c. Foundation and structural design of all commercial and industrial structures shall include consideration of site-specific soils tests and the city's seismic safety (ground acceleration) criteria. Buildings shall be developed in accordance with city-approved plans and specifications including mitigation of soils limitations and seismic hazards.

d. Signs shall conform to the city Sign Regulations except where superseded by specific standards listed in this paragraph. Signs shall be either wall-mounted or low-profile, free-standing.

One free-standing sign at each premises shall be allowed and shall not exceed 12 feet in height or 72 square feet in area.

One wall sign for each business or tenant is allowed on each frontage or building face having a public entrance not to exceed 10 percent of the building face and not to exceed 100 square feet nor to be located above the second story.

Off-premises signs are prohibited within the specific planning area.



## CIRCULATION

### Off-Site Improvements

The most significant off-site circulation change in the vicinity, which would substantially improve both northbound and southbound access to the area from Highway 101, is the Los Osos Valley Road extension from the present interchange across San Luis Obispo Creek to connect with South Higuera Street. This connection, even initially as a two-lane roadway and bridge, would enable traffic from Los Osos and Baywood, as well as the Laguna neighborhood of the city, to cross the present freeway and creek barrier for convenient cross-town travel to the subject area, the county airport, and other locations along or east of Highway 227.

The subdivider(s) of the southern 30 acres shall contribute to the cost of extending Los Osos Valley Road to connect with South Higuera Street. The contribution shall be based on the traffic expected to be generated by the subdivided area as a fraction of total traffic expected to use the new connection. This contribution shall be \$3,640, payable when the final subdivision map for the southern 30 acres is filed.

Additional off-site street improvements which will enhance circulation are listed below in order of anticipated priority and importance. Except as noted, the subdivider/developer of areas within the commerce park will not be required to contribute to funding for these projects.

1. Widening of Higuera Street between Madonna Road and Marsh Street, which would provide a better local access route to and from the Central Business District, without the use of Highway 101. (Adopted plan lines provide for widening, to a total right-of-way width of 84 feet entirely from the west side.)

2. The widening of South Higuera Street south of Madonna Road - South Street intersection to Los Osos Valley Road intersection from previous 60-foot right-of-way to a total width of 84 feet. This improvement is already partially complete and is being accomplished in segments as adjoining properties develop, each providing 12 feet additional right-of-way, related frontage and pavement improvements to conform to adopted plan lines.

3. Widening of Tank Farm Road from its previous 40-foot right-of-way to about 84 feet, between South Higuera Street and Highway 227, (Edna Road). Similar to Higuera widening, this is currently being accomplished in segments as adjoining properties develop, each providing about 22 feet additional right-of-way and street improvements across developing frontage.

4. Widening of Prado Road from its 30-foot prior right-of-way to about 64 feet, particularly west of South Higuera Street to Highway 101. This widening is also being accomplished as adjoining properties develop, each dedicating additional right-of-way across the frontage.

### On-Site Improvements

Higuera Commerce Park development involves widening and improvement of two substantial segments of adjoining arterial and collector streets, as well as internal street construction.

### Prado Road

Prado Road has been widened to an 84 foot right-of-way along the northern part of the specific plan area.

### South Higuera Street and Tank Farm Road

South Higuera Street requires special treatment for several reasons. Higuera Street is a community entrance route and the corridor for access to residential as well as commercial and industrial developments. Further, the east side of the street, south of Prado Road, is lined by a windrow of cypress trees which generally separates the site from the street. The city is encouraging limited access along this route, avoiding driveways and on-street parking where practical. Therefore, the specific plan includes a frontage road parallel to but east of Higuera Street with a 42-foot wide island separating the local access frontage road from the arterial street. The frontage road connects with South Higuera Street at two intersections, but not with either Meissner's Road or Prado Road.

Subdivision improvements for the southern 30-acre special industrial area include widening the east side of South Higuera Street and the north side of Tank Farm Road, along with improving the intersection of these streets for better sight-distance and ease of turning movements.

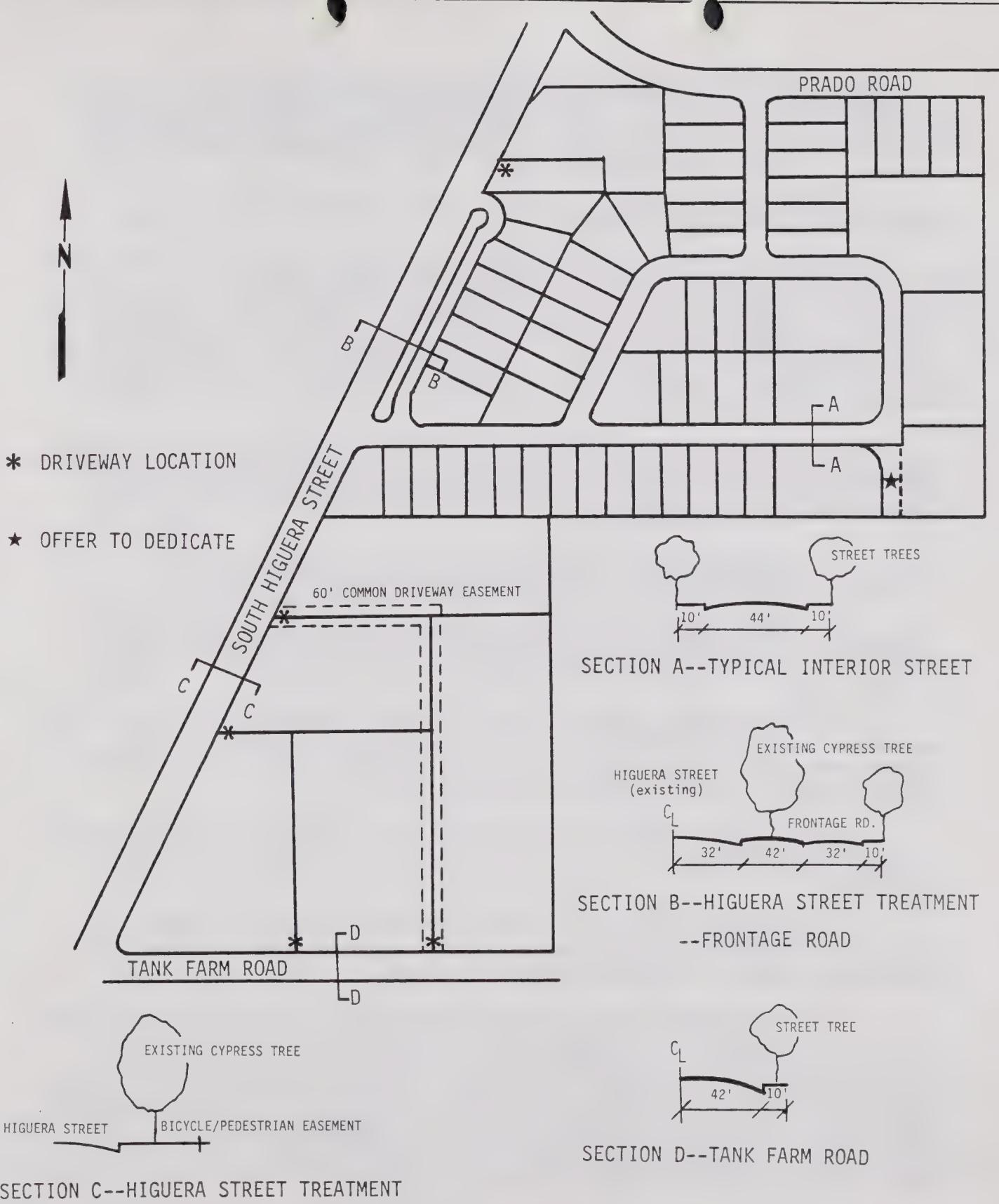
The precise cross-section for widening South Higuera Street is to be stipulated when the final subdivision map is approved. The cross-section shall provide for two travel lanes in each direction and a central turning lane. A bicycle lane shall be included on the east side, preferably along the roadway. If space within the roadway is not sufficient, the bike lane may be provided east of the existing trees. A sidewalk shall be provided east of the existing trees; it may be within an easement.

To minimize traffic conflicts and tree removal, the three parcels fronting South Higuera Street are to be served by two common driveways.

Subdivision of the southern 30 acres will provide for widening of Tank Farm Road (additional 22 feet of right-of-way with curb, gutter, sidewalk, and paving) from the South Higuera Street intersection easterly to the city limits.

In addition to the \$15,000 contribution from the subdivider of the northern 50 acres toward the cost of installing signals at South Higuera Street and Tank Farm Road, the subdivider of the southern 30 acres shall pay \$5,000 when the final subdivision map for that area is filed.

Parcels fronting Tank Farm Road are to be served by common driveways as shown on the Circulation Map. The city engineer may allow driveways at other locations, provided they are at least 120 feet from the intersection or other driveways.



HIGUERA COMMERCE PARK SPECIFIC PLAN

CIRCULATION

Subdivision of the northern 50 acres has provided for cost participation in signalization of the Tank Farm/Higuera intersection, in addition to that which may be required in conjunction with the southern 30 acres. Subdivision of the northern 50 acres also provided some right-of-way dedication at the northeast corner of this intersection.

#### Park Entrances and Interior Streets

The major entrance to Higuera Commerce Park from South Higuera Street is a "T" intersection between Tank Farm Road and Prado Road. This main entrance, approximately one-half mile north of Tank Farm Road and one-third mile south of Prado Road intersection, also enables access to the frontage road parallel to South Higuera Street. On the north, the frontage road intersects South Higuera Street approximately 500 feet south of the Prado Road intersection, providing a secondary entry and exit.

The limited-access character of South Higuera Street shall be maintained. Driveways for the larger parcels near the intersection of South Higuera Street with Prado Road and with Tank Farm Road shall be limited to the locations indicated on the circulation map.

1. Interior local access streets provide 44 feet of pavement within a 64-foot wide right-of-way.

2. Corner radii at the street intersections are 30 feet radius at property line and 40 feet at curb returns to facilitate occasional truck turning, particularly acute or right-angle right turn movements.

3. The frontage road parallel to Higuera Street is 32 feet from curb to curb to accommodate two-way traffic and parking on the east side only. The cypress trees along South Higuera Street remain within a 42-foot wide median island protected on both sides by curbs and gutters.

4. Bike lanes along the east side of South Higuera Street are within the paved street area. To do this, curb-side parking along this frontage is prohibited.

5. Sidewalks are provided along the south side of Prado Road, the east side of the South Higuera frontage road, along the South Higuera Street frontage and along both sides of interior streets.

The sidewalks throughout the development will be 6 feet wide with integral curb, gutter and sidewalk, similar to residential or industrial standards rather than the typical 10-foot commercial width due to limited pedestrian traffic anticipated. This design variation along with street yard requirements will help reduce conflicts with underground utilities such as gas, electricity or phone lines or with street trees behind the sidewalk. Installation of sidewalks on interior streets will be deferred until construction on individual lots is started.

As part of the subdivision improvements for the 30-acre special industrial area, a sidewalk will be provided east of the cypress trees along South Higuera Street. It may be curvilinear.

6. The interior street system as well as the peripheral street intersections create "T" rather than four-way intersections (except where signal-controlled cross-street traffic may be necessary.) The Tank Farm Road and South Higuera Street intersection warrants signalization.

The internal street pattern is designed to discourage through traffic and avoid short-cuts or by-pass movements parallel to major streets.

7. A future street extension of the interior street system is also shown on the circulation map. The extension could allow interconnection with a variety of street patterns which could be developed on adjacent property. The future street extension would be held by the city as permanent "offer of dedication." It would not be developed or used as a street until the city decided to accept the offer of dedication and provide for construction of the necessary improvements to connect to adjoining developments.

Street extension would be made only after amending the city's General Plan to provide for annexation and development of adjacent property. Future street extension is not intended to encourage or induce annexation or development.

8. A common driveway easement within the southern 30 acres would allow eventual resubdivision without additional driveways along exterior streets. While the easement prevents development of buildings which might preclude this internal circulation route, it is not intended to encourage resubdivision, which would have to be preceded by an amendment to this specific plan.



## PUBLIC SERVICES, UTILITIES, AND DRAINAGE

### Water Service

Higuera Commerce Park has water lines within interior streets to form a network adequate for anticipated commercial and industrial uses. This system of looped lines will also provide sufficient fire flow and improve network reliability for existing uses. Local lines are generally 8- or 10-inch, with fire hydrant spacing to comply with city standards (see Utilities and Drainage Map).

A 12-inch diameter water main will be installed along the north side of Tank Farm Road, from Higuera Street east to the city limits, to serve the three large industrial parcels inside the city.

### Sewer Service

Local sewer lines are generally 6- or 8-inch, within street rights-of-way within Higuera Commerce Park (see Utilities and Drainage Map).

### Noise Mitigation

Environmental studies have shown that traffic noise along Prado Road east of South Higuera Street will significantly increase due to the development of the service-commercial district. The mobile home park on the north side of Prado Road will be directly affected by this traffic noise. To reduce noise levels affecting the mobile home park, the project developer will be responsible for the following improvement:

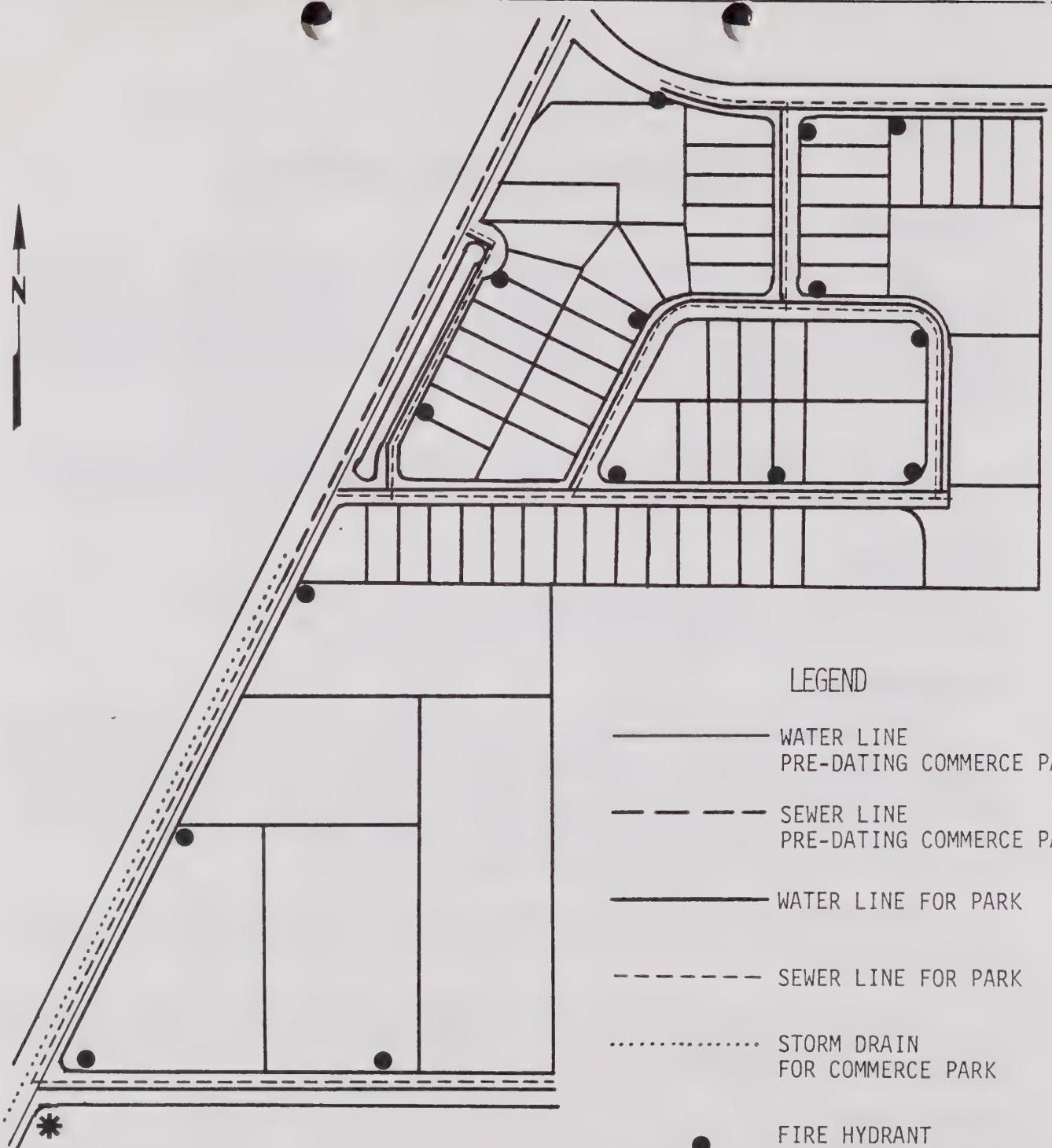
--With authorization from the owner of the mobile home park, the existing wood fence should be replaced with a masonry wall and landscaping to reduce noise levels.

--If authorization from the mobile home park owner is not forthcoming, the city will allow the installation of a masonry wall and landscaping within the public street right-of-way.

### Storm Drainage

Higuera Commerce Park subdivision improvements include correction of two off-site drainage deficiencies by providing an expanded culvert under Tank Farm Road and a pipeline between the Tank Farm Road and South Higuera Street culverts.

Final subdivision improvement plans for Higuera Commerce Park will include on-site drainage improvements needed to supplement surface drainage as well as appropriate off-site drainage facilities. In any event, the drainage improvements of Tract No. 592 and Higuera Commerce Park will remedy drainage problems within this part of the city, but will not reduce drainage problems in the adjoining unincorporated properties to the east and south of the commerce park.



## HIGUERA COMMERCE PARK SPECIFIC PLAN

## UTILITIES AND DRAINAGE

The ponding problem which used to affect only the properties to the east and southeast has apparently been aggravated or displaced by recent grading, and now involves the northeast edge of the site. During 1978 storms, a strip of land 50' to 150' wide along the eastern fence, close to the city limit and site boundary, was flooded about one foot deep. This ponding problem is to be corrected during subdivision construction.

#### Solid Waste, Other Utility and Communications Services

Higuera Commerce Park is provided solid waste collection and disposal services by San Luis Garbage Company. This company, operating under franchise granted by the city, provides both routine and special collection and transports most solid waste to the sanitary landfill at Cold Canyon for disposal.

Southern California Gas Company has a 4-inch main in South Higuera Street, and 3-inch lines on Prado Road and Tank Farm Road, adequate for extension of local lines within street rights-of-way of Higuera Commerce Park, which would enable service to individual commercial or industrial customers. Although such service connections may be discouraged, or if established, may be curtailed due to supply shortages, an adequate underground distribution system to enable full service to qualified commercial and industrial users on any lot will be installed as part of subdivision improvements.

Pacific Gas and Electric Company has overhead power lines along the south side of Prado Road, the west side of South Higuera Street, and the north side of Tank Farm Road as well as two lines across the middle of the site, one along Meissner's Road and the other across the side and rear of an excluded parcel to the north. The lines within Higuera Commerce Park, and the overhead lines along Prado Road will be placed underground. PG&E will also serve underground circuits connecting new street lights.

Four large overhead cables on poles along the east side of South Higuera Street will be converted to underground with the remainder of Pacific Telephone Company's existing and proposed telephone service lines within Higuera Commerce Park. Cable television service will also be installed underground as part of subdivision development, enabling individual industries and businesses to connect to Sonic Cable TV facilities, eliminating the need for outside antennae. Gas, electric, phone and cable television facilities are generally located underground near the edge of public street rights-of-way or within adjoining front yard easements.

#### Police and Fire Protection Services

Police and fire protection services are provided by the city. The Police Department operates from a modern central station located at Santa Rosa Street and Highway 101, and constantly patrols throughout the city. The South Higuera Street area is regularly patrolled. Higuera Commerce Park will provide for security during site planning and architectural design.

Fire protection service is substandard for portions of South Higuera Street south of Prado Road because all city fire stations are more than four minutes away.

Thus, on-site fire suppression (such as sprinklers, fire retardant construction or special alarm systems) will be required.

The fire response time problem can be corrected if Los Osos Valley Road is extended across San Luis Obispo Creek to connect with South Higuera Street. This will provide a direct route to the area from the Laguna Fire Station located at Madonna Road and Los Osos Valley Road.

#### Park and School Facilities

Generally, the South Higuera area lacks convenient access to public parks and schools. This deficiency is aggravated by the incomplete circulation system, which does not allow convenient connection to the present and planned recreational and educational facilities within the Laguna area west of Highway 101. Generally, commercial and industrial uses have no direct need for parks or schools, which are considered residential neighborhood facilities.

## SUBDIVISION PLAN AND IMPLEMENTATION

### Schematic Subdivision Plan

The land use, circulation, public services, utilities, and drainage systems previously described contribute to the schematic subdivision plan for Higuera Commerce Park. A schematic subdivision is shown on the Land Use Map and Circulation Map.

### Phasing

To assure that creation of new jobs will not cause inmigration to overstrain the local housing market and city services, these phasing provisions shall apply:

--The six larger lots at the eastern part of the northern 50 acres shall not be resubdivided unless the city determines a substantial fraction of the smaller lots have been developed and the subdivider demonstrates a need for more small lots which cannot be met elsewhere.

--Within the southern 30 acres, no building permits shall be issued until January 1, 1984. After that date, the city shall not issue building permits for development of more than 15% of the area (4.5 acres) within any one calendar year. Developed area shall include building coverage plus parking lots, outdoor assembly or storage areas, and any required yards.

### Plan Interpretation

Although the specific plan is more detailed in its description of intended development than the general plan, and in most respects is more precise than city-wide zoning or subdivision standards, questions will arise regarding intent.

The Community Development Director shall interpret this plan, subject to the city's appeal procedures.

The allowed and conditional uses listed within the specific plan, as well as the property development standards, supersede conventional zoning provisions. If the specific plan does not contain provisions regarding conditions controlled by conventional zoning and subdivision regulations, it shall be interpreted that the conventional standard is effective.

Variation from standards, except as specifically allowed by provisions of this plan, may be considered pursuant to procedures and findings as defined in the Zoning Regulations. Similarly, wherever the plan provides for conditional use permit, the procedure for obtaining such permit shall be defined in the Zoning Regulations.

To determine the precise location of a land use boundary, whether a specific use is included, or what exact dimension is intended within the specific plan, more accurate maps or standard specifications approved by the city may be used. In such disputes, the decision of the Community Development Director shall be final unless appealed.

The schematic subdivision plan and the circulation, utility and drainage improvement proposals are based on preliminary surveys and engineering. The specific plan is intended to allow subdivision which conforms to the plan's property development standards and street and lot configurations. Lot size and shape are designed to enable two or more adjoining lots to be combined into larger sites by use of standard-form recorded agreement acceptable to the city. Division of any approved lot, or lot line adjustments, may be approved by the city subject to procedures of the Subdivision Regulations, provided that the applicable resultant parcels conform to the special property development standards prescribed by the plan.

If the Community Development Director decides an interpretation would change the intent of this adopted specific plan, he/she should advise the person seeking the interpretation to apply for a specific plan amendment. The Planning Commission, City Council, or any person may initiate specific plan amendments.





